

**SECTION III  
SHADYSIDE STUDY  
AREA**

## **SHADYSIDE STUDY AREA**

### **Introduction**

The Shadyside area is characterized by a unique development pattern which sets it apart from the rest of the borough. The purpose of this study is to acknowledge the area's unique characteristics and promote a comprehensive design which will provide a theme identifying the district. The recently relocated River Road and reconfiguration of that roadway has served to highlight the area's need for its own particular identity and emphasis regarding its accessibility.

The Shadyside study commenced with the selection of the boundary of the study area and the preparation of a lot line base map of the selected area. At the same time a color-spot aerial photograph was taken of this area, at a scale similar to the lot line base map. Land use data from the tax office was recorded on borough tax maps, the data was field-verified to ensure accuracy, and subsequently recorded on the lot line base map. Building locations, street widths, parking areas, sidewalks, trees and other pertinent physical elements were transposed from the aerial and field maps on to the lot line base map, resulting in a single composite map depicting the area's physical conditions and features.

An existing land use and existing conditions survey of the study area was undertaken as part of this assessment. This survey encompassed a review of the uses which comprise the study area, as well as the relationships of parking, buildings, landscape, signage, lighting, and circulation elements. The existing land use survey is presented in mapped form on an accompanying Existing Conditions Map at the end of this report. A meeting was held with interested Shadyside residents and property owners, at which time the Consultant's data base and preliminary suggestions for revitalization were examined.

### **Study Area**

The Shadyside Study Area is that area bounded by Old River Road, New River Road, Gorge Road, and Thompson Lane. A secondary study area, located to the north of the Shadyside area, was also included.

The selected boundaries of the study area are depicted on the accompanying map. The area is bound generally by Gorge Road to the south and west and New River Road to the east and the public parking area to the north. The area encompasses approximately 19 acres.

## Overview of Planning Analysis

The analysis of the area's existing conditions reveals a number of particularly noteworthy features which merit attention. These include both typical features which are traditionally found in mixed-use districts as well as a number of nontraditional characteristics which are particular to Shadyside.

One's initial impression is that the Shadyside area is typified by a distinct land use arrangement. This is exemplified by the development pattern along Old River Road, which includes a combination of small lot sizes, side line to side line building arrangements, compatible building heights, narrow road width and sidewalk dimensions, and the existence of some incompatible land uses such as auto repair garages, etc. The fully developed land use arrangement along the street, coupled with specific types of retail and service commercial uses in the area serves to reinforce the unique character of this area.

This district also contains a number of features which detract from the traditional downtown character and design of the district, thereby diminishing the overall integrity and design of the area, and adversely affecting the local shopping experience. These critical elements relate to the area's physical size, the circulation pattern serving the area, the development pattern (including lot sizes and arrangement), the established design elements, pedestrian accessibility, types of stores, parking, and related items which, when taken together, affect the perception of the district as a pleasant and attractive place to work, shop, or visit. The areas of concern include the following:

1. **Size and Orientation:** The Shadyside area is only 19 acres in area, and its established design is typified by two distinct segments. These include those properties fronting on Old River Road which represent the majority of the area, and properties fronting on Gorge Road. The latter area is typified by a more spread-out character which is not as pedestrian-oriented as those lots fronting on Old River Road.
2. **Road Network Serving the Area:** There are three County roads and one local street serving the district. Unfortunately, the realignment of River Road area has affected access to this area. New River Road, the new main thoroughfare, provides access to the perimeter of the district, but does not direct traffic to the main shopping area. Consequently, none of the stores benefit from the type of pass-by traffic which has traditionally been provided by River Road.
4. **Lack of a Focal Point/Anchor:** The Shadyside area does not have an apparent focal point which draws shoppers to the area. Although this area has historically been known as an artists' community, there is not an identifying designation for the area, nor is there a single store or use in this area which is a magnet for people or, more critically, which can draw other stores who feel they can feed off of the drawing power of the magnet store or use.

4. **Lack of an Enhanced Physical Character:** The study area is characterized by a nominal architectural style and discordant design elements, and lacks any unifying physical focal point. Buildings generally lack a distinct architectural motif. The existing street lighting fixtures are more common to highway lighting than an attractive retail setting. There is limited landscape amenity within the area.
5. **Limitations on Pedestrian Circulation and Orientation:** This area lacks a substantial pedestrian orientation. The pedestrian circulation does not provide a circular flow which directs people through the area. In some instances the sidewalks are too narrow. The intersection of New River Road and Thompson Lane is too wide to enable pedestrians to feel that crossing the area can be accomplished in a safe and efficient manner.
6. **Lack of a Marketable Theme:** The Shadyside area has a distinctive identity in the marketplace which can be utilized to distinguish it from other shopping areas and districts. However, its artistic roots have not been captured in any thematic expression or physical identity which emphasizes the area's unique artistic character.

These represent some of the planning and design issues which are evident, and which require planning responses if the Shadyside area is to fulfill its promise.

### **Plan Goals**

The Shadyside Study Area Plan is a general guide for the physical, economic and social development of this area. It is prepared in accordance with the New Jersey Municipal Land Use Law, and is designed to guide the development of the Shadyside area in a manner which promotes the public health, safety and general welfare. The following enumeration presents broad planning goals which form the basis for the planning and design recommendations which comprise the Plan component of this report.

1. To establish a heightened sense of community in Shadyside by creating a cohesive and attractive mixed-use district.
2. To provide a comprehensive and coordinated long-range plan to guide the growth, development, and physical improvements which are necessary to ensure the continued vitality of Shadyside.
3. To expand the commercial base, as well as the residential component of the area, with an emphasis on the arts, associated retail sales and service uses serving the residents of the community and immediate surrounding area.
4. To enhance the physical appearance of this district through comprehensive and integrated streetscape improvements, building renovations, and related physical improvements.
5. To encourage the cooperation of merchants, property owners, residents and government in

the overall revitalization of the Shadyside area.

6. To encourage public and private investment in the area.
7. To encourage uses and activities which will bring vitality to the area and an increased pedestrian presence.

### **Shadyside Area Plan Recommendations**

The accompanying Shadyside Conceptual Site Plan maps and associated detail sheets provide a schematic representation of the recommendations offered for the physical, visual and aesthetic improvement to this area. The primary purpose of these recommendations is to provide a specific planning direction, and a visual depiction of the manner in which the district's development could occur in a complementary integrated format, while reinforcing the applicable goals and objectives of the Borough of Edgewater Master Plan. These recommendations are designed to serve as a blueprint to guide the revitalization of the Shadyside area for years to come, in recognition of the community benefits which result from a well-thought out plan which provides an overall direction for the physical improvement of the this area.

The five accompanying maps, provided at the end of the report, are entitled Primary Study Area - Suggested Improvements, Study Area Map, Entrance of Thompson Lane, Vignette #1-View Southbound, Vignette #2 -View of Northbound Entrance. These maps depict a variety of physical improvements to the downtown area, including proposals for streetscape elements, landscape features, parking designs, outdoor seating areas, vehicular circulation improvements, focal points, pedestrian circulation elements, etc. These and related planning recommendations are detailed below.

1. **Creation of a Focal Point:** Many successful business and commercial districts have a distinctive feature which immediately identifies the district. It could be a unique architectural element, townsquare, sculpture, etc. It is suggested that the borough needs to create a community feature(s) which will serve to establish a distinctive physical element for Shadyside, and also represent a point of entry to the district. Three specific planning features are recommended:
  - a. The first recommendation is to create visual focal points and/or thematic entrance features for emphasis of the neighborhood entrance at the intersection of New River Road and Thompson Lane. The proposal is depicted on the "Entrance to Thompson Lane" map.
  - b. The next recommendation is to provide an attractive, unique gateway entrance feature at the intersection of Thompson Lane and Old River Road. These features will attract attention to the area from the surrounding roadways and help to anchor the theme of the district.

The map identified as "Vignette #1 - View Southbound", depicts the provision of a center median within Thompson Lane which could serve as a possible location for the display of an artistic sculpture. The island also provides an area for additional landscaping to visually buffer the expanse of pavement at this entrance. A series of sidewalk connections is also provided to the island to delineate and emphasize the movement of pedestrian activity.

- c. The third recommendation regards the Gorge Road/Old River Road intersection. This area is delineated on a map entitled "Vignette #2 - View Northbound. It is suggested that this area be enhanced by the provision of the following: appropriate street lighting, additional gateway entrance features, a formalized sidewalk circulation feature which introduces a pedestrian feature to this street, and the encouragement of an outdoor dining area at the River Gorge Cafe.
2. Streetscapes: The term "streetscape" is a design term referring to those elements that constitute the physical makeup of a street and that, as a group, define its character. It includes such features as street paving, street furniture, landscaping, signage, lighting, and sidewalk details.

The following recommendations offer specific design elements for Shadyside.

- a. It is suggested that the sidewalk area located in front of the Mayger property on the southwest corner of Thompson Lane and River Road be extended eastward. This expansion will improve pedestrian movement and will create recessed on-street parking southward along River Road. The expansion will also allow for outdoor dining to occur in front of the existing and proposed buildings.;
- b. The provision of street shade trees to supplement existing plantings is recommended. The proposal calls for the placement of a number of street trees along the roadways. The proposal calls for the use of a variety of trees (shown on the accompanying plant schedule) which are disease resistant and which will complement the adjacent buildings.

**TABLE 21  
SUGGESTED SHADE TREES  
SHADYSIDE AREA  
BOROUGH OF EDGEWATER, NEW JERSEY**

BOTANICAL NAME	COMMON NAME	SIZE CAL	MIN. HT.	REMARKS	LOCATION
ACER GINNALA "FLAME"	AMUR MAPLE	2"	10- 12'	B&B*, TREE FORM	STREET TREE, PARKING LOTS
AMELANCIER AUTUMN BRILLIANCE	AUTUMN BRILLIANCE AMELANCHIER	2"	10- 12'	B&B, TREE FORM	STREET TREE, PARKING LOTS
PYRUS CALLERYANA "CAPITAL"	CAPITAL PEAR	2½ - 3"	13- 14'	B&B	STREET TREE, PARKING LOTS
PRUNUS SARGENTI COLUMNARIS	COLUMNAR SARGENT CHERRY	2½ - 3"	10- 12'	B&B	STREET TREE, PARKING LOTS
ZELKOVA SERRATA GREEN VASE	GREEN VASE JAPANESE ZELKOVA	2½ - 3"	13- 15'	B&B	STREET TREE, PARKING LOTS

\*B & B = Balled & Burlaped

- c. It is suggested that landscape screening be incorporated along with a decorative wall and/or fence to screen the public parking area located on the north side of Thompson Lane between Old and New River Roads. As depicted on the drawing entitled "Primary Study Area - Suggested Improvements", a decorative wall or fence should be placed adjacent to the parking area and continue along Thompson Lane westward to the intersection of Old River Road. It is recommended that a portion of the municipal property, near the pump house, be developed for display of art and for seasonal gardening. The purpose of these features is to make the statement that one is entering a special district serving the community. This would be reinforced with an appropriate identification sign announcing the district.
- d. Landscaping should also be added along the westerly edge of the public parking area to screen, but not totally obscure, the adjacent parking and to add seasonal interest.
- e. Another improvement is the provision of decorative lighting fixtures which will replace the existing lighting fixtures presently situated along the street. The existing fixtures are completely out of context to the building and pedestrian scale of the area, and do not provide the quality of lighting which is necessary to encourage vibrant and safe

evening activity. The proposal calls for the use of single-and double-lighted fixtures, installed at a height of twelve feet. The height and design of these fixtures serve to reinforce the pedestrian scale of the district. These fixtures serve to establish a specific tone, visual element, and character to the district. Examples of these fixtures are shown on some of the accompanying pages.

- f. It is recommended that consideration be given to the provision of outdoor cafes to enhance the character of the district, and provide increased activity in the area, particularly in the evening hours. This will make the area more vibrant, and broaden its market attraction.
  - g. The plan also calls for the provision of contrasting decorative brick pavers at the intersection of Old River Road and Thompson Lane. The accompanying sketches depict the placement of a stylized paving field which will define the pedestrian areas.
  - h. Use of Sidewalk Area. It is recommended that the sidewalk area be limited to flower planters along the building facade, benches and cafe seating (only permitted in front of storefronts by a revocable permit, and at intersections), and decorative garbage cans located next to shade trees. Other material, leaflets on poles, etc, would detract from the overall design theme and character of the district, and should not be permitted.
  - i. Street/Directional Signs. Accompanying detail sheets depict decorative street identification signs which should be placed at intersections. These signs are designed to complement the design style of the above noted elements to reinforce the overall design theme of the area. The signs should be utilized to inform patrons of the location of parking areas, as well as street identification.
  - j. Streetscape furniture (benches, trash receptacles, bike racks, etc.) should be added for additional convenience of shoppers and to add style to the district. Examples are provided on accompanying detail sheets.
3. **Parking and Pedestrian Circulation:** The availability and access to parking for customers, employees, and commuters is always a critical issue facing a business district, and Edgewater is no exception. An analysis reveals that there are 160 public parking spaces available in the Shadyside area, including on-street parking and the public parking area. Presently, there is a need for 180 parking spaces based on the existing number of dwelling units and the square footage of non-residential development in Shadyside. The total buildout of all vacant properties along the west side of Old River Road would require a total of 220 spaces. Consideration should be given to requiring on-site parking for new developments to alleviate the shortfall in this area.

In an effort to improve pedestrian safety in the Shadyside district, clearly delineated crosswalks are recommended. These crosswalks, depicted on various sketches in this report, would assist in defining pedestrian rights-of-way. The crosswalks should be

supplemented with signage which indicate that pedestrians have the right-of-way.

4. Development of Vacant Sites: There is one specific area which merits attention. It is the 3.0± acre tract located to the south of Thompson Lane between Old and New River Roads. It is suggested that in order to complement the architectural mass and scale of the buildings located on the west side of Old River Road, a maximum four story building height be permitted at this location. In order to reinforce the building wall along River Road it is recommended that a limited front yard setback be permitted and that the front of the facade face Thompson Lane and Old River Road, as delineated on maps “ Vignette #1”, “Entrance of Thompson Lane” and “Primary Study Area - Suggested Improvements”. Parking for the proposed building should be placed to the rear of the structures with landscaping and/or a decorative wall to screen the on-site parking from New River Road.

Building materials and details should be compatible to the existing buildings in Shadyside and shall be applied to all visible sides of the building. The facade treatment facing New River Road should have a “front” appearance in order to maintain a visual relationship with that roadway and other nearby developments along River Road.

5. Architectural Details, Facades and Related Elements: Architectural facades and related elements encompass such issues as the treatment of the building face, use of awnings, entrance detailing, commercial signs, and window treatments. The manner in which these items are addressed will determine the visual image that Shadyside presents to the public. The following suggestions offer direction for enhancing the buildings in the district by encouraging complementary design elements that provide for visual and aesthetic continuity. These suggestions are not directed towards creating one particular style but are intended as a general guideline.
  - a. A coordinated signage system should be imposed as a unifying element. The use of signage made of compatible materials, muted colors such as earth tones with a limitation on the number of colors (typically two or three colors are permitted), size (generally sign width should not be more than seventy to eighty-five percent of the storefront's width, depending on the size of the store, and letters should not be more than approximately two to two and a half feet in height), and regulations on elevations may be utilized to complement the prevailing character of development. It is suggested that the particular characteristics of the Shadyside area lends itself to more expressive and unique signage provisions than the rest of the borough's commercial districts. All of these suggestions should be incorporated into the local development ordinance. It is noted that these guidelines should not be construed as limiting diversity or creativity. Rather they should be viewed as encouraging a certain degree of compatibility and scale, leaving individual merchants the freedom to creatively adopt standards to meet their own personal requirements.
  - b. Covered or blocked windows should be discouraged through the local development ordinance, because they tend to impose a negative impression to the area. Display

windows should extend across the entire facade. No more than a specified percentage of window area (usually no more than fifteen to twenty percent) should be permitted to be covered by signage.

- c. Building and design elements such as wood trim features, color, and window lighting elements often serve as a unifying architectural and design theme. Typically, brighter color hues should only be utilized to accentuate details, rather than serve as a dominant feature of a building. Earth tones should be utilized for the sides and rears of buildings in order to visually soften and unify the area. The incorporation of subdued color schemes often minimizes the obtrusiveness of buildings in relation to their surroundings.
- d. The use of complementary and related architectural facade features should be encouraged. The use of building indentations for larger facades to break up large blank walls, quoining on building corners, cornice work, complementary color schemes, lighting, etc all serve to reinforce a specific character. The local development ordinance could limit the total amount of an area of any given facade without architectural relief to a specified linear dimension or percentage.
- e. Building maintenance remains the most elemental form of rehabilitation which reinforces the character of a commercial area and the pride that the merchants take in their community. Cleaning and repointing brickwork, repairing windows, and replacing obsolete signs and deteriorated surface materials can all represent significant visual improvement.

### **Implementation**

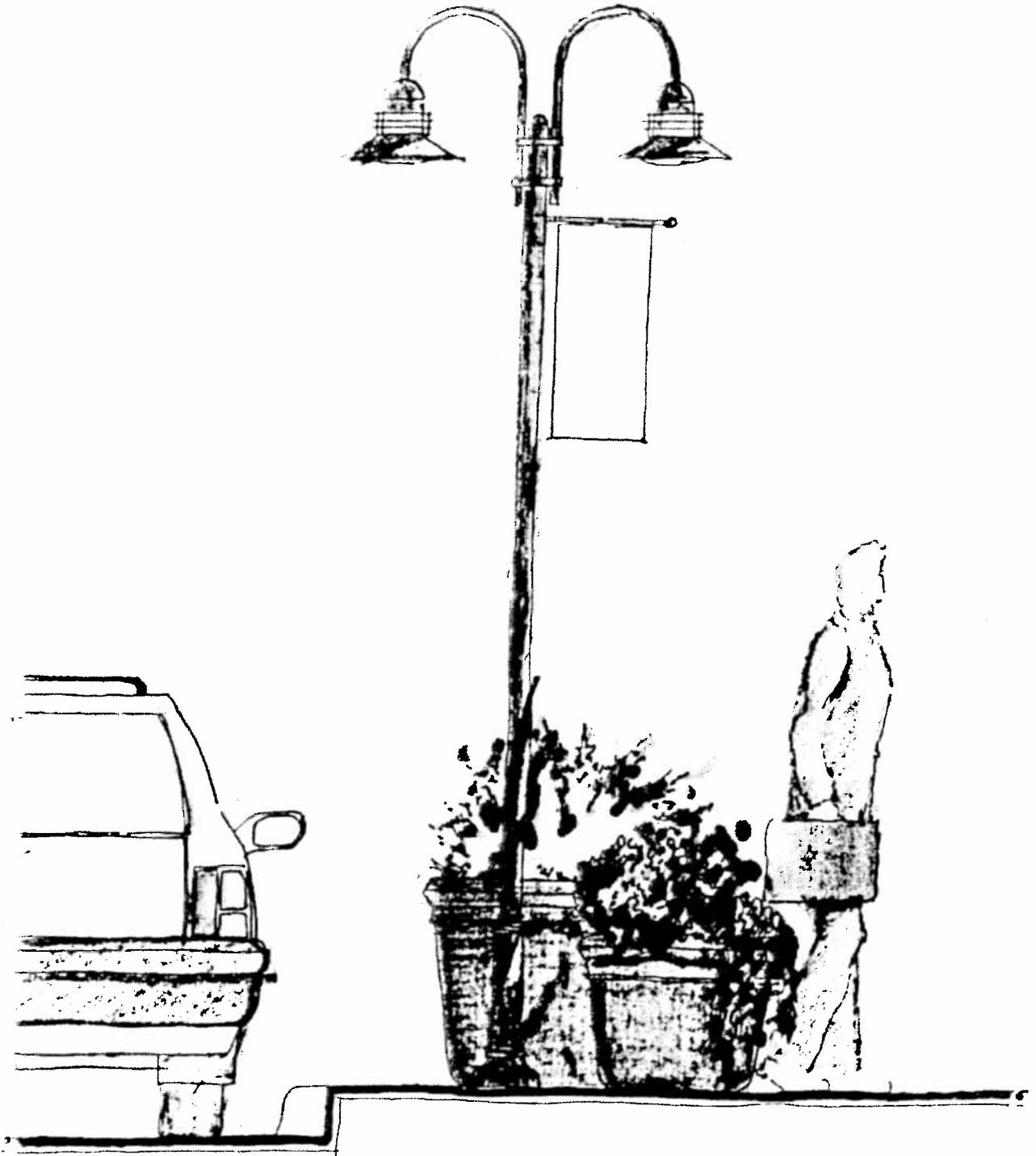
In order to initiate and administer the improvement of properties in the business district which are set forth in this report, a standardized public process of review and approval must be established in conjunction with programs for financial and technical assistance. Several possibilities are enumerated below.

1. The Borough Zoning Ordinance should be evaluated to assess consistency with these guidelines, and where necessary it should be amended so that it reaffirms the policies and guidelines set forth herein. This would also ensure that the implementation of this document is consistent with the Municipal Land Use Law S.40:55D-1 et. seq. Local ordinances should also reflect policy decisions to encourage activities which draw people to the area in the evening hours, increase night-time hours of operation, and pursue policies which encourage more restaurants and outdoor cafes.
2. Design assistance and technical advice should be offered to applicants during the course of development application processing and site plan review, utilizing the guidelines set forth herein.

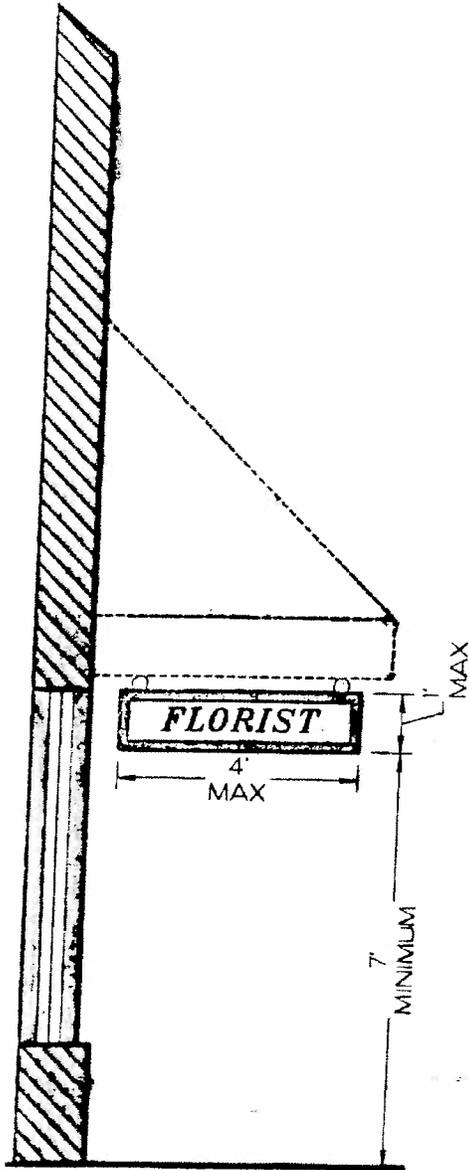
3. The business district should retain the services of a company who is responsible for marketing the district, the same way in which shopping malls rely on coordinators to market malls. A marketing events coordinator could serve the community by encouraging business to the area, focusing on the extraordinary retail trade dollar that this area represents, and encouraging various activities which make Edgewater's business district a destination for the arts and for consumers.
  
5. The business district and municipality should actively pursue relationships with local banks to encourage their participation in low interest loan programs. Additionally, the borough should actively pursue any available funding source that may possibly be utilized in assisting in the implementation of the streetscape improvements called for in this plan. The following funding possibilities are noted:
  - a. Community Development Block Grants. This program, with money available both from Bergen County and the State, funds public facility improvements, economic development, and affordable housing projects. The program provides grants up to \$350,000 per project per year, and may be used to undertake streetscape improvements called for in this Plan. The main requirement of the program, however is that more than half of those benefiting from the grant must be lower income individuals.
  
  - b. Special Improvement Districts. Special Improvement Districts (SID) are areas designated by municipalities for special improvements or services that are paid for by a separate tax on property owners in the district. To encourage the use of SIDs, the Department of Community Affairs (DCA) has created a program to help finance the start-up costs of these districts. This program provides up to \$10,000 in loans to a municipality, said amount repayable without interest over a five-year term. The municipality must provide one dollar in funding or in-kind services for every two dollars loaned from DCA. SIDs can, however, be created by municipal ordinance without the funding assistance offered by the State.
  
  - c. Main Street New Jersey. The National Trust for Historic Preservation sponsors a State-operated Main Street New Jersey program aimed at assisting municipalities in undertaking downtown improvement and revitalization projects. The program does not provide direct funding assistance, but instead provides advisory and administrative guidance in developing downtown programs. Participants are instructed on how to develop a comprehensive approach to downtown planning, including design, promotion, fund raising and economic restructuring. The program encourages volunteerism, special function committees and matching funding by the participating merchants and municipalities.

Municipalities in the Main Street program are required to hire a full-time downtown manager for a period of at least three years. Municipalities must contain "historic architectural resources" to be eligible for the program.

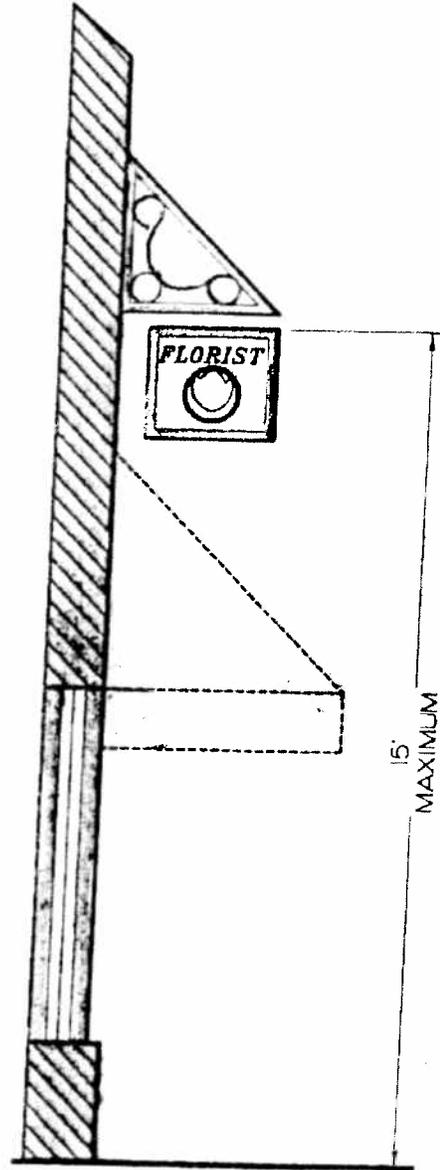
- d. New Jersey Neighborhood Empowerment Program. This program, administered through the Urban Coordinating Council (UCC), provides the state's urban centers with opportunities to create functional public/private partnerships, to leverage funds for entrepreneurial growth and jobs creation, and enhance empowerment. Municipalities that participate are eligible for investments in infrastructure improvements and other development projects. The funding source is the New Jersey Redevelopment Authority (NJRA) of the New Jersey Redevelopment Investment Fund.



Drawing Name STREET LIGHT STANDARDS	Project No. 96-450-05	Drawing Date 8.22.99	Addendum No. X , X
 <b>BURGIS ASSOCIATES, INC.</b> PROFESSIONAL PLANNERS / LANDSCAPE ARCHITECTS 25 WESTWOOD AVENUE WESTWOOD, N.C. 27175 (919) 468-1811 FAX (919) 468-2539	Project Name EDGEWATER EXHIBITION SANDHOLE PARKWAY	Drawing Scale NTS	Sheet No. X , X Drawing No. X



SECTION  
- HANGING SIGN / AWNING



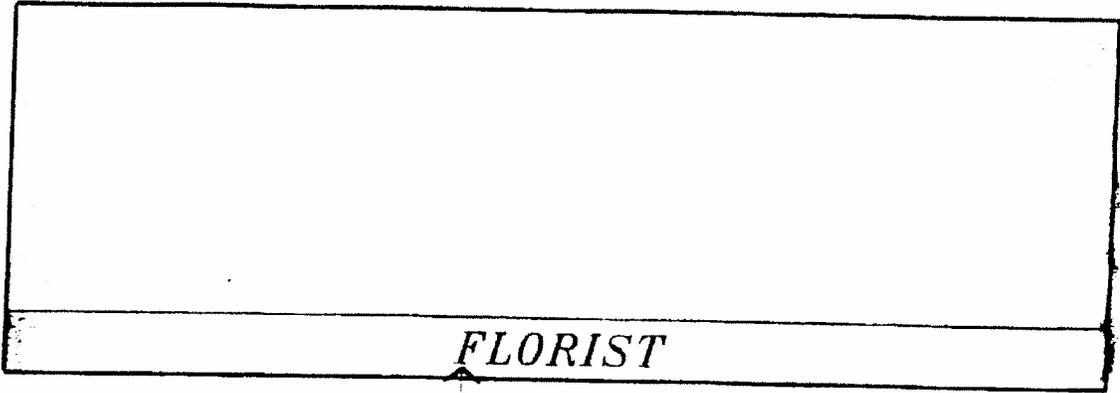
SECTION  
- HANGING SIGN / BUILDING BRACKET  
ABOVE AWNING



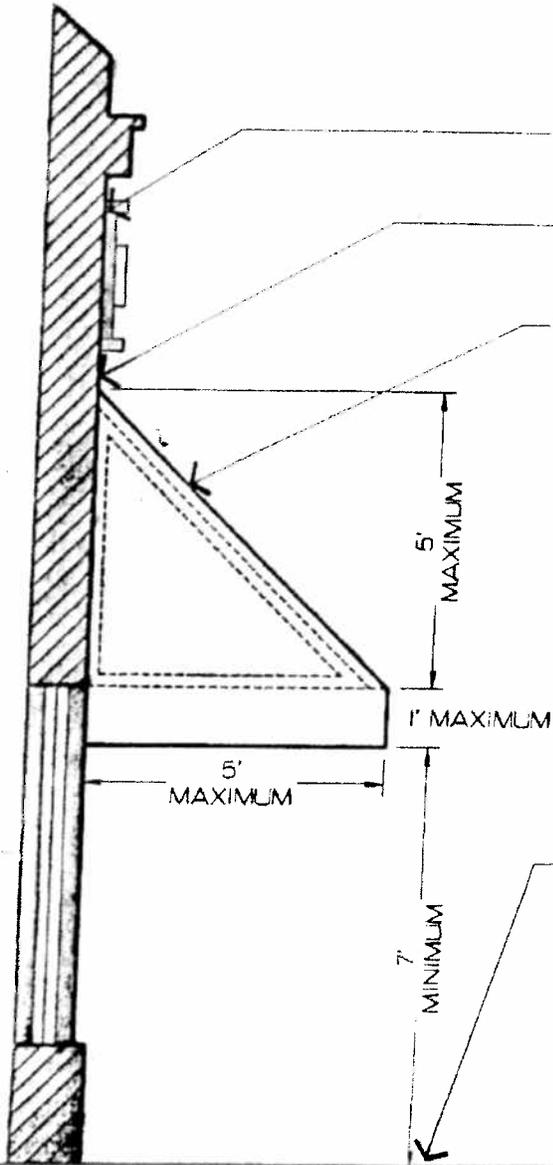
HANGING SIGNAGE

BOROUGH OF EDGEWATER

NOT TO SCALE



ELEVATION



FRONT VALENCE SIGN

WALL SIGN

STORE FRONT

AWNING:

SIGNAGE: - FRONT VALENCE

MAXIMUM TEXT SIZE SIX INCHES  
IN HEIGHT. TEXT TO OCCUPY NO  
MORE THAN TWO THIRDS OF THE  
TOTAL AWNING LENGTH.

- ROOF AREA

NOT PERMITTED

MATERIAL: WOVEN FABRIC OR APPROVED EQUAL

SHAPE: ANGLED SLOPING ROOF OR  
APPROVED EQUAL

SUPPORTS: PERMANENT WELDED TUBULAR FRAME CONSTRUCTED  
IN ACCORDANCE WITH INDUSTRY STANDARDS  
ATTACHED SECURELY TO BUILDING

EXISTING GRADE

SECTION



AWNING SECTION

BOROUGH OF EDGEWATER

NOT TO SCALE